

NOMINATED SUBCONTRACT FOR THE DESIGN BUILD, OPERATIONS AND MAINTENANCE OF THE TOLL SYSTEMS FOR THE OPERATIONS AND MAINTENANCE OF TOLL PLAZAS ON THE N2 TSITSIKAMMA TOLL ROAD

CONTRACT SANRAL N.002-089-2021/1-NSC

TENDER CLARIFICATION BRIEFING

**SCM: SANRAL
PROJECT MANAGER: ROLAND THOMPSON
CONSULTANT: TPO CONSULTING**

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AGENDA

- Introduction
- Housekeeping
- Description of the Project
- SCM Presentation:
 - PART T1: Tendering Procedures
 - PART T2: Returnable Schedules
 - PART C1: Contract Data
- Technical Presentation:
 - PART C3: Scope of Work
- Questions
- Close

TENDER BRIEFING FOR CONTRACT SANRAL N.002-089-2021/1-NSC

NOMINATED SUBCONTRACT FOR THE DESIGN BUILD, OPERATIONS AND MAINTENANCE OF THE TOLL SYSTEMS FOR THE OPERATIONS AND MAINTENANCE OF TOLL PLAZAS ON THE N2 TSITSIKAMMA TOLL ROAD

- Introduction
 - Take over and maintain existing Toll System
 - Design and Implement new Toll System
 - Develop, test, roll out SANRAL MIS Interface – if triggered
 - Develop, test and roll out TCH interface
 - Develop, test and roll out ITIS Interface
 - Develop, test and roll out Banking Interfaces
 - Operations and Maintenance of Toll System
 - Tender for MIN 37 months, up to 73 months Operations including Design Build
 - SANRAL Project Manager: Roland Thompson
- Housekeeping
 - Attendance Register
 - Form A1
 - Form A1.1
 - Plaza visits: procurementsr4@sanral.co.za

DESCRIPTION OF THE PROJECT

The Tsitsikamma Toll Plaza is situated on the N2 Tsitsikamma Toll Road which forms part of the National Route 2 that runs between Cape Town in the Western Cape, through Durban in KwaZulu Natal to Ermelo in Mpumalanga. The N2 Tsitsikamma Toll Road starts in the Western Cape province on the National Route 2 Section 8 at Soutriver and extend to Bloukrans River (km 97.28) and the border with Eastern Cape province. From the border at Bloukrans River bridge (km 66.10) it follows National Route 2 Section 9 in the Eastern Cape province up km 82 at the Witelsbos turn off. The total distance of the toll route amounts to 110 km.

The standard of the Tsitsikamma Toll Road varies from a dual carriageway freeway with three lanes in each direction to an undivided dual carriage freeway with two lanes in each direction.

There is one mainline Toll Plaza on the Tsitsikamma Toll Road and one set of ramp plazas, and the general details are summarised as follows:

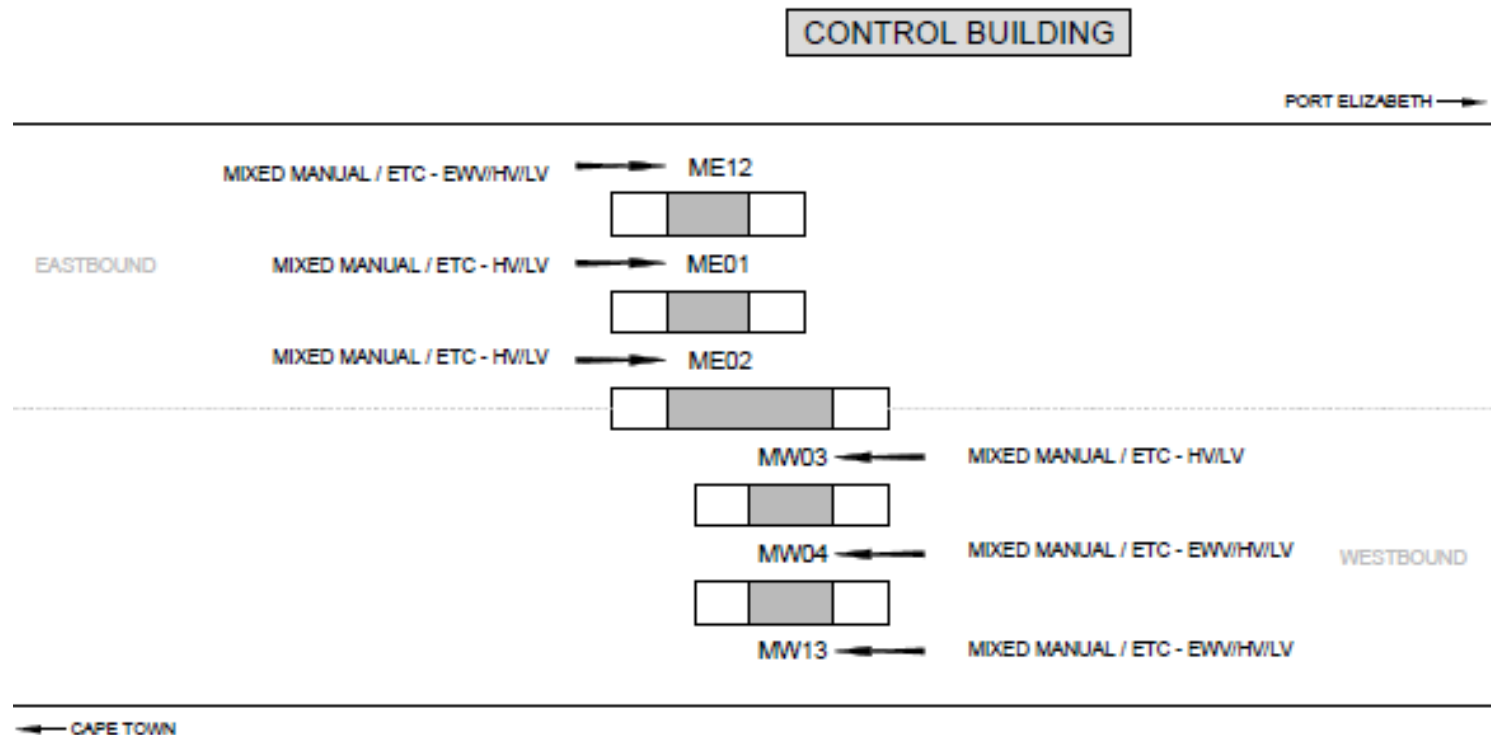
Toll Plaza	Control Centre	Location	Section Chainage	km	Number of Lanes*
Tsitsikamma Mainline	Tsitsikamma	N2	8	94.10	3/3
Tsitsikamma Ramps	Tsitsikamma	N2	8	93.70	1/1

*x/y denotes per direction (Northbound/Southbound)

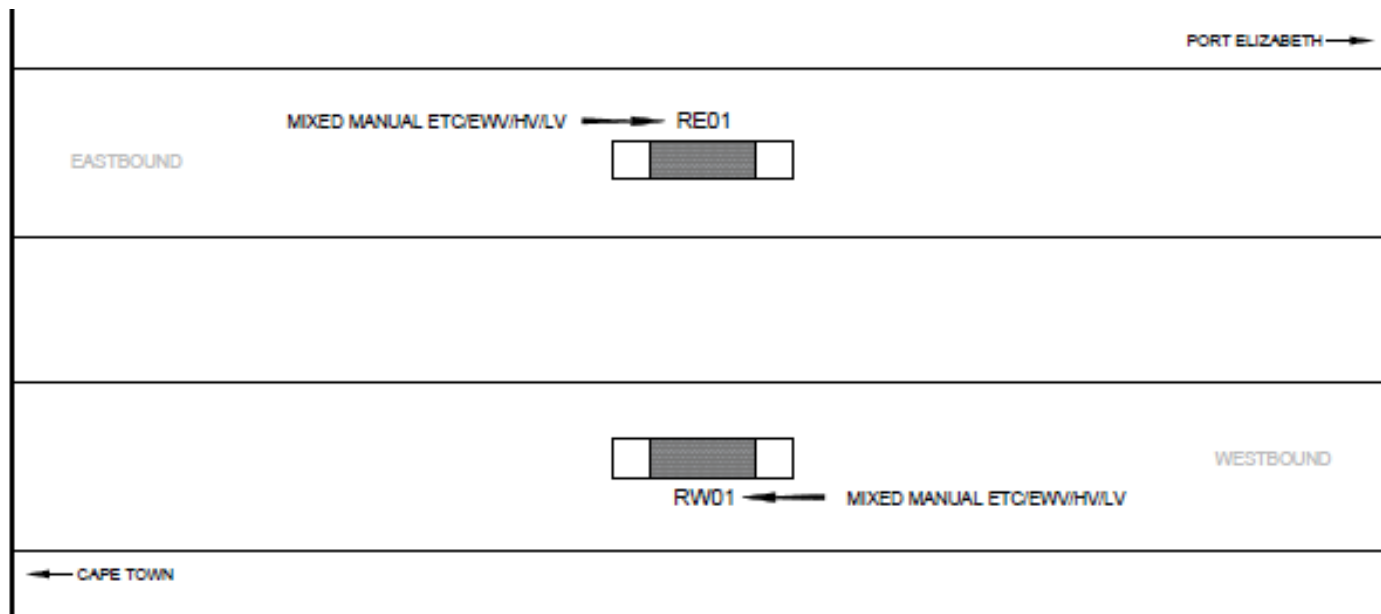
DESCRIPTION OF THE PROJECT



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SCM PRESENTATION

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SCM PRESENTATION CONTENT

- Tender Document Structure
- SANRAL's Preferential Procurement Strategy
- Conditions of Tender
- T.1.2 Tender Data
- Part T2: Returnable Schedules
- Tender Evaluation Scoring
- Part C1: Contract Data

TENDER DOCUMENT STRUCTURE

VOLUME-3	PROJECT DOCUMENT
PART-T1	TENDERING PROCEDURES
T1.1	Tender Notice and Invitation to Tender
T1.2	Tender Data
PART-T2	RETURNABLE SCHEDULES
T2.1	List of Returnable Schedules
T2.2	Returnable Schedules
PART-C1	AGREEMENT AND CONTRACT DATA
C1.1	Form of Offer and Acceptance
C1.2	Contract Data
C1.3	Particular Conditions -- Part B: Special Provisions
C1.4	Other Standard Forms
PART-C2	PRICING DATA
C2.1	Pricing Instructions
C2.2	Schedule of Payments/Cost Matrix
PART-C3	SCOPE OF WORK
C3.1	Project Description
C3.2	Particular Specifications for Operations and Maintenance
C3.3	Environmental Management Plan
C3.4	Occupation Health and Safety
C3.5	Risk Assessment Plan
PART-C4	SITE INFORMATION
PART-D	Stakeholder and Community Liaison and Targeted Labour and Targeted Enterprises Utilisation and Development
PART-E	ANNEXURES
VOLUME-4	PROJECT INFORMATION DOCUMENT

OTHER DOCUMENTS OF INTEREST

- Conditions of Tender: Annexure C of the CIDB Standard For Uniformity In Engineering And Construction Works Contracts
- Volume 1: Contract for Design, Built and Operate Projects (2008) published by Fédération Internationale des Ingénieurs-Conseils (FIDIC)
- Volume 2: Employer's Requirements Standard Specifications

PREFERENTIAL PROCUREMENT

GENERAL CONDITIONS

- The applicable preference point system for this tender is the 90/10 preference point system.
- Points for this tender (even in the case of a tender for income-generating contracts) shall be awarded for:
 - (a) Price; and
 - (b) Specific Goals.

PREFERENTIAL PROCUREMENT

1.1 To be completed by the organ of state:

The maximum points for this tender are allocated as follows:

	POINTS
PRICE	90
SPECIFIC GOALS	10
Total points for Price and Specific Goals	100

PREFERENTIAL PROCUREMENT

Table 1: Specific goals for the tender and points claimed are indicated per the table below.

The specific goals allocated points in terms of this tender	Criteria	Number of points allocated (90/10 system)	Number of points allocated (80/20 system)	Number of points claimed (90/10 system) (To be completed by the tenderer)	Number of points claimed (80/20 system) (To be completed by the tenderer)
B-BBEE level scorecard of the tendering entity.	B-BBEE Level 1	1,00	2.00		
	B-BBEE Level 2	0,90	1.80		
	B-BBEE Level 3	0,60	1.20		
	B-BBEE Level 4	0,50	1.0		
	B-BBEE Level 5	0,40	0.80		
	B-BBEE Level 6	0,30	0.60		
	B-BBEE Level 7	0,20	0.40		
	B-BBEE Level 8	0,10	0.20		
	Non-compliant contributor	0	0		
Percentage black ownership of the tendering entity.	< 51 % black ownership	0	0		
	≥ 51 to < 70 % black ownership.	1	2		
	≥ 70 to < 100 % black ownership.	2	4		
	100 % black ownership	5	10		
Percentage Subcontracting to Targeted Enterprises	Min 50 % subcontracting	0	0		
	> 50 to < 55 % subcontracting	1	2		
	≥ 55 to < 60 % subcontracting	2	4		
	≥ 60 % subcontracting	4	8		

PREFERENTIAL PROCUREMENT

Points for specific goals will be awarded according to the table below:

1. The tenderer's scorecard shall be a B-BBEE Certificate issued in accordance with:
 - the amended Construction Sector Codes published in Notice 931 of 2017 of Government Gazette No. 41287 on 1 December 2017 by the Department of Trade and Industry; or
 - in the event that the Measured Entity operates in more than one sector or a sub-sector, the scorecard for the sector or sub-sector in which the majority of its core activities (measured in terms of annual revenue) are located will be acceptable. The tenderer must comply with the annual revenue thresholds for EME or QSE or Generic in accordance with the amended Construction Sector Codes; and
- i. The scorecard shall be submitted as a certificate attached to Returnable Schedule Form C1; and
- ii. The certificate shall:
 - be valid at the tender closing date; and
 - have been issued by a verification agency accredited by the South African National Accreditation System (SANAS); or
 - be in the form of a sworn affidavit or a certificate issued by the Companies and Intellectual Property Commission in the case of an Exempted Micro Enterprise (EME) with a total annual revenue of less than R3 million if issued in accordance with the amended Construction Sector Codes published in Notice 931 of 2017 of Government Gazette No. 41287 on 1 December 2017 by the Department of Trade and Industry; and
 - have a date of issue less than 12 (twelve) months prior to the original advertised tender closing date (see Tender Data C.2.15); and

PREFERENTIAL PROCUREMENT

- i. A valid BBBEE Certificates shall contain:
 - Name of enterprise as per enterprise registration documents issued by CIPC, and enterprise business address.
 - Value-Added Tax number, where applicable.
 - The B-BBEE Scorecard against which the certificate is issued, indicating all elements and scores achieved for each element. The actual score achieved must be linked to the total points as per the relevant Codes.
 - B-BBEE status with corresponding procurement recognition level.
 - The relevant Codes used to issue the B-BBEE verification certificate.
 - Date of issue and expiry (e.g. 9 June 2018 to 8 June 2019). Where a measured entity was subjected to a re-verification process, due to material change, the B-BBEE Verification Certificate must reflect the initial date of issue, date of re-issue and the initial date of expiry. Re-verification does not extend the lifespan of the B-BBEE Verification Certificate.
 - Financial period which was used to issue the B-BBEE Verification Certificate.
- i. A valid Sworn Affidavit shall contain:
 - Name/s of deponent as they appear in the identity document and the identity number.

PREFERENTIAL PROCUREMENT

Designation of the deponent as either the director, owner or member must be indicated in order to know that person is duly authorised to depose of an affidavit.

- Name of enterprise as per enterprise registration documents issued by the CIPC, where applicable, and enterprise business address.
 - Percentage black ownership, black female ownership and whether they fall within a designated group.
 - Indicate total revenue for the year under review and whether it is based on audited financial statements or management accounts.
 - Financial year-end as per the enterprise's registration documents, which was used to determine the total revenue. **The valid format of the Financial Year-End is Day/Month/Year**
 - B-BBEE status level. An enterprise can only have one status level.
 - Date deponent signed and date of Commissioner of Oath must be the same.
 - Commissioner of Oath cannot be an employee or ex officio of the enterprise because, a person cannot by law, commission a sworn affidavit in which they have an interest, and
- i. Compliance with any other information requested to be attached to Returnable Schedule Form C1; and
 - ii. In the event of a Joint Venture (JV), a project-specific consolidated (SANRAL project number indicated) valid B-BBEE verification certificate in the name of the JV, issued by a verification agency accredited by the South African National Accreditation System (SANAS) shall be submitted.

PREFERENTIAL PROCUREMENT

1. Black ownership
 - i) The bidder must complete Form A14: Black ownership declaration. (A14 Appendix E)
 - ii) *Verification of ownership will be based on share certificate. SANRAL reserves the right to engage with the shareholders.

1. Sub-contracting
 - i) Tenderers must complete Form A15: Sub-contracting declaration. (A15 Appendix F)

Criteria for breaking deadlock

If two or more tenders score the same number of points and these tenders are also the highest ranked tenders, the tender with the highest preference points will be recommended for award.

If functionality is part of the evaluation process and two or more tenders score equal total points and equal preference points, the tender that scored the highest points for functionality will be recommended for award.

If two or more tenders score the same number of financial points and preference points and these tenders are also the highest ranked tenders, the tenderer to be recommended for award will be decided by the drawing of lots.

PREFERENTIAL PROCUREMENT

- PART D: Stakeholder and Community Liaison
- Specification: PART D of PART C3
- Structured engagement with project Stakeholders and affected Communities.
- Guides the selection and the enhanced utilisation and development of Targeted Labour and Targeted Enterprises from the Target Area(s).
- Key players
 - Project Liaison Committee (PLC)
 - Project Liaison Officer (PLO)
 - Project Management Team (PMT)
 - Stakeholders
 - Targeted Enterprise Procurement Coordinator (TEPC)
 - Target Group Development Coordinator (TGDC)
- Contract Participation Performance (CPP)
 - Incentives / Penalties

CONDITIONS OF TENDER

C.2.8	<p>Seek clarification</p> <p>Request clarifications at least 12 working days before the closing date.</p>
C.2.16	<p>Tender offer validity</p>
C.2.16.1	<p>The tender offer validity period is 24 weeks.</p>
C.3.7	<p>Grounds for rejection and disqualification</p> <p>Prior to disqualification, the Employer shall inform the tenderer and give the tenderer an opportunity to make representations within 14 (fourteen) days as to why the tender submitted should not be disqualified and as to why the tenderer should not be restricted by the National Treasury from conducting any business with any organ of state for a period not exceeding 10 years.</p> <p>In the event of disqualification, the Employer may, at its sole discretion, claim damages from the tenderer and impose a specified period during which tender offers will not be accepted from the offending tenderer and the Employer shall inform the National Treasury.</p>

CONDITIONS OF TENDER

4. **SUBMISSION OF TENDER** – of the contract documents, only the following elements of Volume 3 needs to be submitted in a neatly bound file and in the following order:
- a) **Technical Proposal (USB Flash Drive and printed and bound hard copy enclosed in one Envelope)**
 - All Returnable schedules listed in T2.1 List for Returnable Schedule for inclusion in the Technical Proposal.
 - The Returnable Schedules shall be completed in the electronic format stipulated in T2.1 List for Returnable Schedule and stored in a USB Flash Drive and printed and bound hard copy.
 - Where T2.1 List for Returnable Schedule specify submission in a format other than PDF, the specific Returnable Schedule shall be included in both PDF and the stipulated format and stored on the USB Flash Drive.
 - b) **Financial (USB Flash Drive/CD enclosed in one Envelope)**
 - All Returnable schedules listed in T2.1 List for Returnable Schedule for inclusion in the Financial Proposal.
 - The Returnable Schedules shall be completed in the electronic format stipulated in T2.1 List for Returnable Schedule and stored in a USB Flash Drive.
 - Where T2.1 List for Returnable Schedule specify submission in a format other than PDF, the specific Returnable Schedule shall be included in both PDF and the stipulated format and stored on the USB Flash Drive.

Both envelopes shall be sealed in one envelope and submitted in accordance with instructions as stated in the Tender Data.

Information provided by a tenderer over and above the above elements of volume 3 shall be treated as information only and will only be bound into the document if the tenderer notes on Form A4: Schedule of Variations or deviations that the information has a bearing on the tender price.

5. For alternative offers the tenderer shall comply with instructions of item 4.

EVALUATION CRITERIA

- Technical Proposal:

Item	Functional Scoring	Form	Max Score
1	Relevant Experience of the Tendering Entity	D5.1 & D5.2	50
2	Tendering Entity's QMS Credentials	D14	10
3	Participation of Key Previously Disadvantaged Individuals (PDI) Staff	D14	10
4	Key Resources (CV's to be provided)	D4 & D14	60
5	Tendering Entity's Proposed Design, Build, Software Development and Operations and Maintenance Plan	D14	55
6	Technical and System Information	D10	72
	Total Score		257
	Score (Minimum = 75%)		192.75

EVALUATION CRITERIA

- **Financial Proposal:**
- The Financial proposal shall not be opened, unless the tender achieves a minimum score of 75% for the Quality evaluation
- Financial Maximum Score – 90 points.
 - $P_s = 90 \times (1 - (P_t - P_m / P_m))$
- Specific Goals Maximum Score – 10 points

PART T2

RETURNABLE SCHEDULES

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PART T2: RETURNABLE SCHEDULES

- Fully completed relevant returnable documents
- Failure may render such a tender offer non-responsive – Addendum 2
- Use Form F1 and Table T2.1 List for Returnable Schedule as a guide and checklist
- In the case of a Joint Venture (JV) or Consortium, a separate returnable is to be completed and submitted by each member for a range of the returnable documents:
 - A2.1 - A2.6, A3.1 - A3.4, A6, A8, A9.1, A9.2, A10, A11
- Form C1.1. –JV specific B-BBEE Certificate

PART T2: RETURNABLE SCHEDULES

- Make sure signatures are provided where required
- All declarations must be true, correct and signed.
- C.1.1.1 Form of Offer– Make sure it is signed and offer agree with Total in Part C2.2
- C1.2.3 Information provided by Tenderer – make sure it is complete
- C2.2 Pricing Schedule/Cost Matrix – Submit Excel Spreadsheet
-

PART C1 CONTRACT DATA

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PART C1: CONTRACT DATA

For the Main Contract the following shall apply:

1. The **General Conditions** of Contract shall be the "Conditions of Contract for Design, Build and Operate Projects, First Edition 2008" published by the Fédération Internationale des Ingénieurs-Conseils (FIDIC)
2. The Contract Data (Particular Conditions, Part A) as set out in the Main Contract Part C1, and which provisions take preference over the General Conditions and Special Provisions; and
3. The Special Provisions (Particular Conditions, Part B) as set out in the Main Contract PART C1, and which provisions take preference over the General Conditions.
4. A copy of the Main Contractor's Tender Document will be available on SANRAL's website to the NSC for purposes of context and a better understanding of the Works.

For the Nominated Sub-Contract (NSC) (this Contract) the following shall apply:

1. The General Conditions of Contract shall be the "Conditions of Contract for Design, Build and Operate Projects, First Edition 2008" published by the Fédération Internationale des Ingénieurs-Conseils (FIDIC)
2. The Contract Data (Particular Conditions, Part A) as set out in C.1.2.1 below, and which provisions take preference over the General Conditions and Special Provisions; and
3. The Special Provisions (Particular Conditions, Part B) as set out in C.1.2.2 below, and which provisions take preference over the General Conditions.
4. A copy of the Nominated Subcontract will be made available to the MC for purposes of context and a better understanding of the Works.

PART C1: CONTRACT DATA

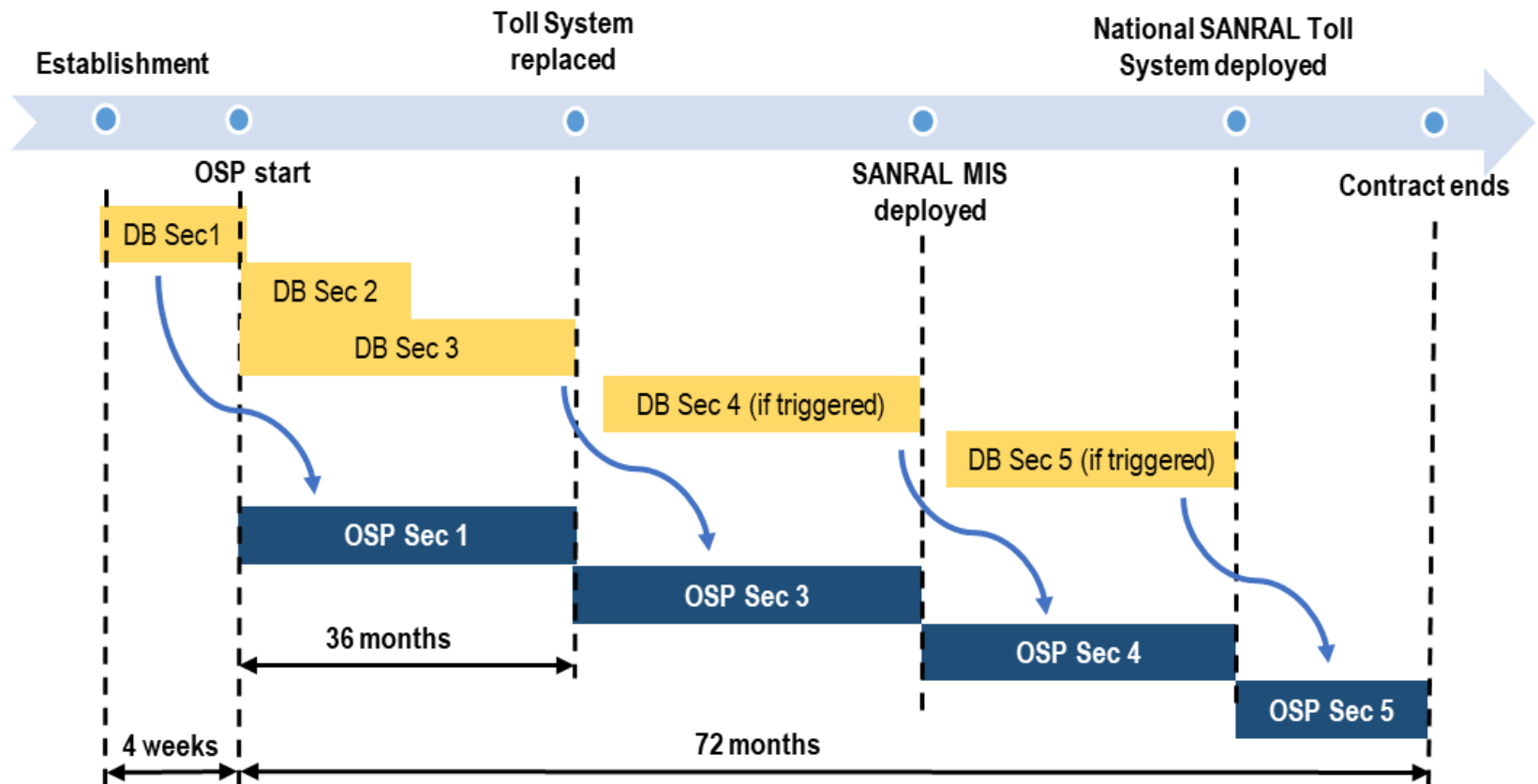
The terminology used for the contracts and the various roles versus the FIDIC terminology is clarified below

Contract Entity	Main Contract (MC) (CTROM Operations and Maintenance Contract)	Toll System Subcontract (NSC)	How these should be interpreted in this contract
SANRAL	Employer	Principal Employer	Principal Employer
Main Contractor (MC) (Operations and Maintenance Contract)	Contractor	Employer	Main Contractor
Toll System Subcontractor (NSC)	Nominated Subcontractor (NSC)	Contractor	Nominated Subcontractor (NSC)
Employer's Representative	Employer's Representative	Main Contractor's (MC) Representative	MC Representative
SANRAL MIS System Integrator			3rd Party- SANRAL MIS System Integrator (SI)
SANRAL Toll System Integrator			3rd Party- SANRAL Toll System Integrator

PART C1: CONTRACT DATA

- Design Build - Work divided into sections, to be triggered by Employer - Appendix 1
- 4.2: 2 x Performance Securities:
 - DB: 10% Design Build Part of Contract Amount
 - OPS: 10% Operation Service Part of Contract Amount.
- 5.2: Contractor's Documents requiring Employer approval – Appendix 2
- 8.2: Time for completion – ref diagram and table Appendix 1
- 9.6: Delay damages – Appendix 1
- 14.3: No Retention

PART C1: CONTRACT DATA



DB – Design-Build
OSP – Operations Service Period
Sec – Section

PART C1: CONTRACT DATA

Section no	Design-Build Section definition	Section Start date	Time for completion (days)
Sub-Clause references		1.1.6, 8.1, 9.1	1.1.78, 8.2; 9.2
1	Establishment period.	Commencement Date	28 days
2	Keep existing compliance level: Functional Compliance with the 2010 CTROM requirements plus Establishment Upgrades	Section 1 Commissioning	180 days
3	The supply of a complete Toll System, including additional items detailed in V3B1. (This section runs in parallel with Section 2 and contains the following sub-sections 3.1 to 3.9)	Section 1 Commissioning	1080 days
4	The deployment of the SANRAL MIS interface. (If triggered item).	Date stated in trigger Notice	180 days
5	The deployment of the national SANRAL Toll System. (If triggered item)	Date stated in trigger Notice	240 days

TECHNICAL PRESENTATION

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SCOPE OF WORK

- **General:**

- Continuation with current Toll System
- Assets
 - Provision, installation and maintenance of Employer's Facilities and Permanent DB (Toll System related)
 - Provide and maintain Contractor's Facilities and equipment (Toll System related)
- Involvement of Targeted Enterprise(s)
- Main Contractor provides non-Toll System related Assets, including Help Desk, Asset Management, Electrical and Mechanical, etc
- Nominated Subcontractor responsible for Toll System

SCOPE OF WORK

C3.1.2.1 Description of Roles and parties to this Contract

The Employer's procurement strategy includes tenders for two contracts simultaneously:

1. The first contract will be for the Main Contractor for the Operations and Maintenance of Toll Plazas on the N2 Tsitsikamma Toll Road (also known as the CTROM contract).
2. The second contract will be for the Nominated Subcontractor for the Design Build Operations and Maintenance of the Toll System for the Operations and Maintenance of Toll Plazas on the N2 Tsitsikamma Toll Road .

Once the Employer has selected the two contractors; the Main Contractor will be instructed to appoint the Nominated Subcontractor.

This Contract is the CTROM Contract for the Operations and Maintenance of Toll Plazas on the N2 Tsitsikamma Toll Road.

The terminology used for the contracts and the various roles versus the FIDIC terminology is tabled and clarified below.

PART C1: CONTRACT DATA

The terminology used for the contracts and the various roles versus the FIDIC terminology is clarified below

Contract Entity	Main Contract (MC) (CTROM Operations and Maintenance Contract)	Toll System Subcontract (NSC)	How these should be interpreted in this contract
SANRAL	Employer	Principal Employer	Principal Employer
Main Contractor (MC) (Operations and Maintenance Contract)	Contractor	Employer	Main Contractor
Toll System Subcontractor (NSC)	Nominated Subcontractor (NSC)	Contractor	Nominated Subcontractor (NSC)
Employer's Representative	Employer's Representative	Main Contractor's (MC) Representative	MC Representative
SANRAL MIS System Integrator			3rd Party- SANRAL MIS System Integrator (SI)
SANRAL Toll System Integrator			3rd Party- SANRAL Toll System Integrator

SCOPE OF WORK

- Toll System Design and Build
- Toll System Operate and Maintain
- Support Collection of toll revenue (through Toll system)
- Support Traffic management
- Maintenance of toll system and related assets
- Facilitate system and operational auditability through reporting

SCOPE OF WORK

- **Assets**
- Employers Facility (fixed assets, equipment, documents)
 - Contractor responsible for day-to-day maintenance.
 - Contractor to repair these assets and cost to be recovered from the Employer on 3 quotations system.
- Permanent Design-Build Assets (plant assets, material, Contract documents)
 - Contractor to maintain, repair, replace, upgrade, provide or dispose of these assets.
 - Contractors' Facility and Equipment
- Contractor facilities and Contractor equipment
- Existing Asset Register listing all the assets were provided in Part C4 of Volume 4.

Asset category	
Employer's facilities	Employer's fixed assets
	Employer's equipment
	Employer's documents
Permanent design-build assets	Plant assets
	Materials
	Contractor documents
Contractor's facilities	
Contractor's equipment	

SCOPE OF WORK

- **Split of risk – Form D13**
- Form D13 defines a Risk Transfer Model, i.e. split of risk between Contractor and NSC
 - MC responsible for NSC management
 - Operator will operate and have access to health monitoring data and VGS audit tool
 - NSC will maintain and upgrade, and ensure toll system optimal performance and KPI compliance
 - Data variance is handled by Payment Methodology (V2B7a), including AVC detection and classification issues
 - Responsibility to ensure optimal system performance both MC and NSC, focused on early detection
 - Health monitoring and statistical performance assessment
 - Configuration control ensure that NSC do not modify system (Changes reported by health monitoring system)
 - MC supply help desk, ERS and stock control
 - NSC supply spares, maintain and keep spares above minimal levels. Replenish at end of contract.
- NSC carries main risk related to toll system aspects. **Main Contractor carries a small portion of risk, as Contractor remains responsible for overall management of NSC.**

SCOPE OF WORK

- Toll system (Design build sections – V3B1: C1.2.3 APPENDIX 1 and Table 5-1: System upgrades)
- Establishment Period (DB Sec 1) – 4 Weeks
- Establishment upgrades (DB Sec 2) – 6 Months
- Upgrade/ replacement of equipment (DB Sec 3) – 36 Months (start same time as Sec 2)
- Interface to the SANRAL MIS (DB Sec 4) – 6 Months
- SANRAL Toll System (DB Sec 5) – 8 Months
- End of contract upgrades and handover

SCOPE OF WORK

- Toll collection lane equipment
- Standard 2010 system with enhancements including:
 - TCH interface (PGW on site, via route and GORT fibre)
 - Contactless bank card (EMV) processing (PCI compatible) readers. Proof before roll-out
 - ANPR: Pilot followed by deployment
 - VGS audit role:
 - Incident and missing/duplicate transaction
 - AVC count accuracy
 - Concession/Discount verification (ANPR/VLN discrepancies)
 - Health monitoring (Response by NSC, with or without MC reporting) and configuration control
 - PCI level 2 merchant implementation
- SANRAL MIS interface (Basic TCH type interface) and further enhancements
- SANRAL Toll System (Assessment, selection, pilot and roll-out) [SANRAL access to source code]

SCOPE OF WORK

- TCH interface
 - Move TCH PGW to OPS level at a plaza on the route (S2, establishment)
 - Utilize National and GORT fibre networks

SCOPE OF WORK

Employer's MIS interface

- Deployment of a centralised SANRAL MIS to which Toll System must interface
- Employers MIS Interface (DB Sec 4) – 6 Months
 - Basic
 - Full (WA)
- Payment item
 - Payment item B-2016 will be removed
 - Allowed for in provisional sum B-6010

SCOPE OF WORK

National SANRAL Toll System

- Employer will select the system from 1 of the existing CTROM routes
- SANRAL Toll System (DB Sec 5) – 8 Months
- Payment item B-2014
 - Allocate 3% of national source code cost to this route
 - Item B-2014h (Full Toll System Intellectual Property and ownership) removed
- SANRAL Toll System procurement process (Tendered rates)
 - Assessment of NSC systems
 - Selection of system to be deployed and access to source code (for use or to own fully)
 - Test and evaluation phase (In parallel on selected routes)
 - Central NSC contract (Use tendered rates per route)
 - Development and test environment Pilot deployment (parallel deployment) at a site
 - Route roll-out and decommissioning of NSC system
- SANRAL Toll System NSC provide system and support (Procurement models)

SCOPE OF WORK

- **ITIS**
 - Submit data to ITIS
 - Use ITIS data as part of payment **decision** tree

SCOPE OF WORK

- **Involvement of Targeted Enterprise**
- Requirement to involve Targeted Enterprise (TE) (30% of contract value)

SCOPE OF WORK

- **KPIs**
- The Performance Measurements - Volume 2 Book 6a.
- Clause C3.2.6 from page C3-221 onwards for modifications.
- These modifications are in line with the consolidated KPI Model agreed on the National Performance Review Committee (NPRC).

SCOPE OF WORK

- **Payment methodology**
 - Schedule of Payments/Cost Matrix consist of 3 schedules:
 - Schedule A – Operation Service Period (OPEX).
 - Schedule B – Design-Build Period (CAPEX).
 - Schedule D - Stakeholder And Community Liaison and Targeted Labour

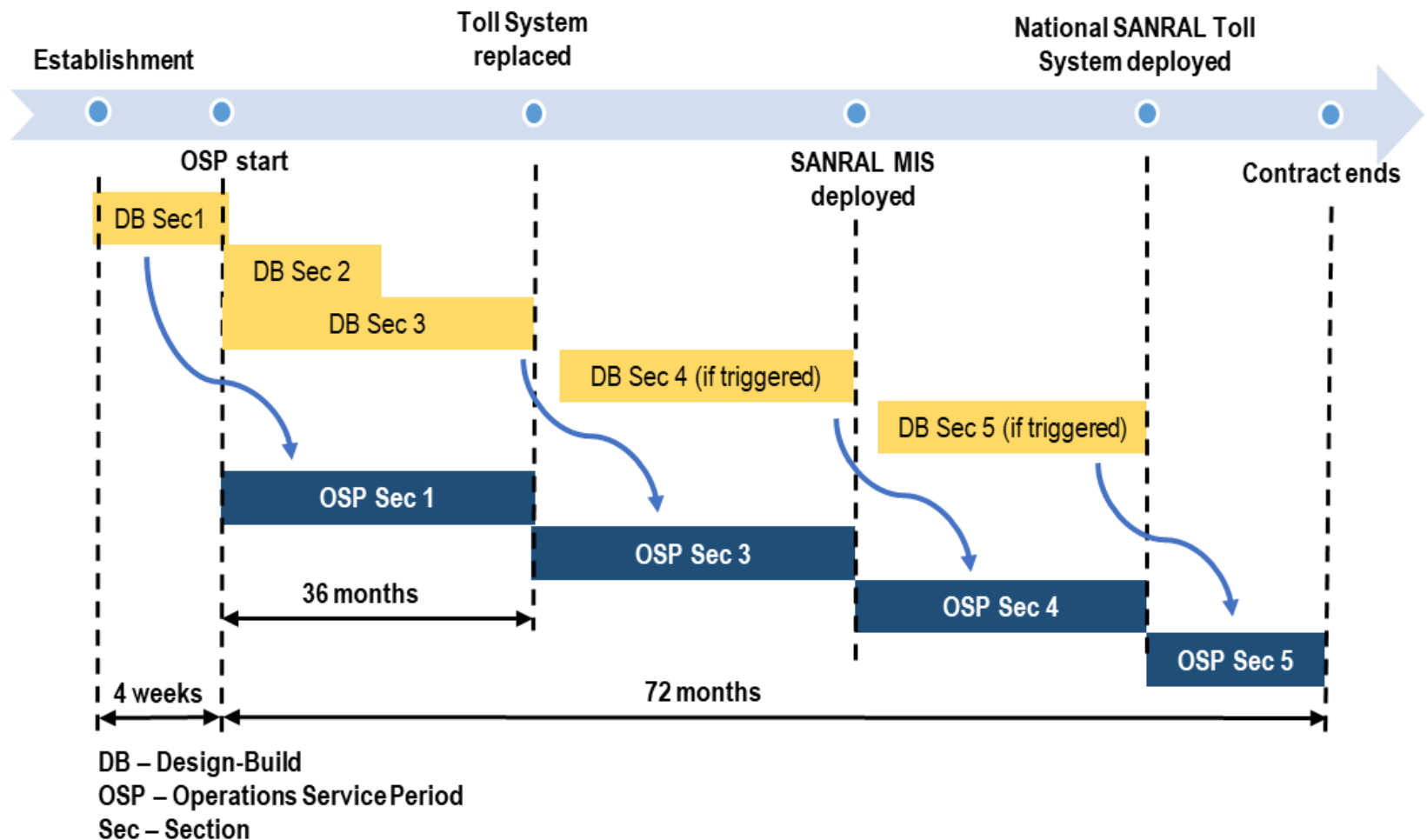
TENDER PROGRAMME

- Tender Advertise 24 February 2023
- Submission of Form A1.1 25 May 2023
- Tenderers meeting (briefing presentation is available on SANRAL website)
- Last day for questions by Tenderers (working days) 6 June 2023 (Closing date minus 12 working days)
- Last day for Employer Addenda (working days) 8 June 2023 (Closing date minus 10 working days)
- Closing date 23 June 2023
- Start of operations service period To be determined depending on the procurement process

PROJECT PROGRAMME

- Effective date
 - Commencement date
 - Access to site for establishment
 - Provision of Performance Bond
 - Start of operations service period and DB period
 - Operation service period
 - Design-Build period (excl S4 & S5)
- Date of Letter of Acceptance
 - Date of Commencement meeting
 - 4 weeks before start of OSP
 - 28 days after letter of acceptance
 - After DB Sec 1 Commissioning
 - Min 37 months, Max 6 years
 - 3 years

PROJECT PROGRAMME



PROJECT PROGRAMME

Section no	Design-Build Section definition	Section Start date	Time for completion (days)
Sub-Clause references		1.1.6, 8.1, 9.1	1.1.78, 8.2; 9.2
1	Establishment period.	Commencement Date	28 days
2	Keep existing compliance level: Functional Compliance with the 2010 CTROM requirements plus Establishment Upgrades	Section 1 Commissioning	180 days
3	The supply of a complete Toll System, including additional items detailed in V3B1. (This section runs in parallel with Section 2 and contains the following sub-sections 3.1 to 3.9)	Section 1 Commissioning	1080 days
4	The deployment of the SANRAL MIS interface. (If triggered item).	Date stated in trigger Notice	180 days
5	The deployment of the national SANRAL Toll System. (If triggered item)	Date stated in trigger Notice	240 days



TSITSIKAMMA
RAMP PLAZA

QUESTIONS?

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